



ROCHELLE PARK

REDEVELOPMENT PLAN

TOWNSHIP OF ROCHELLE PARK, NEW JERSEY



PREPARED BY HEYER, GRUEL & ASSOCIATES
MAY 2014

INTRODUCTION

The revitalization, continued growth, and sustainability of commercial districts and residential neighborhoods are key components of the Township of Rochelle Park’s long term goals. The Township has determined that one of the most effective planning and implementation strategies is the use of the redevelopment and rehabilitation process in accordance with the Local Redevelopment and Housing Law NJSA 40:12A et. seq. Over the years since the previous master planning process was undertaken, the Rochelle Park neighborhood has seen several significant new development projects.

To better complement and sustain this growth and development, the Township has declared this area in Rochelle Park an “Area in Need of Rehabilitation.” The Township Committee designated the Area as an Area in Need of Rehabilitation by resolution dated March 19, 2014 (see Rehabilitation Area map). Rehabilitation areas are similar to redevelopment areas with two key differences; the use of eminent domain is not authorized and tax abatement is limited to 5 years.

Area & Site Description

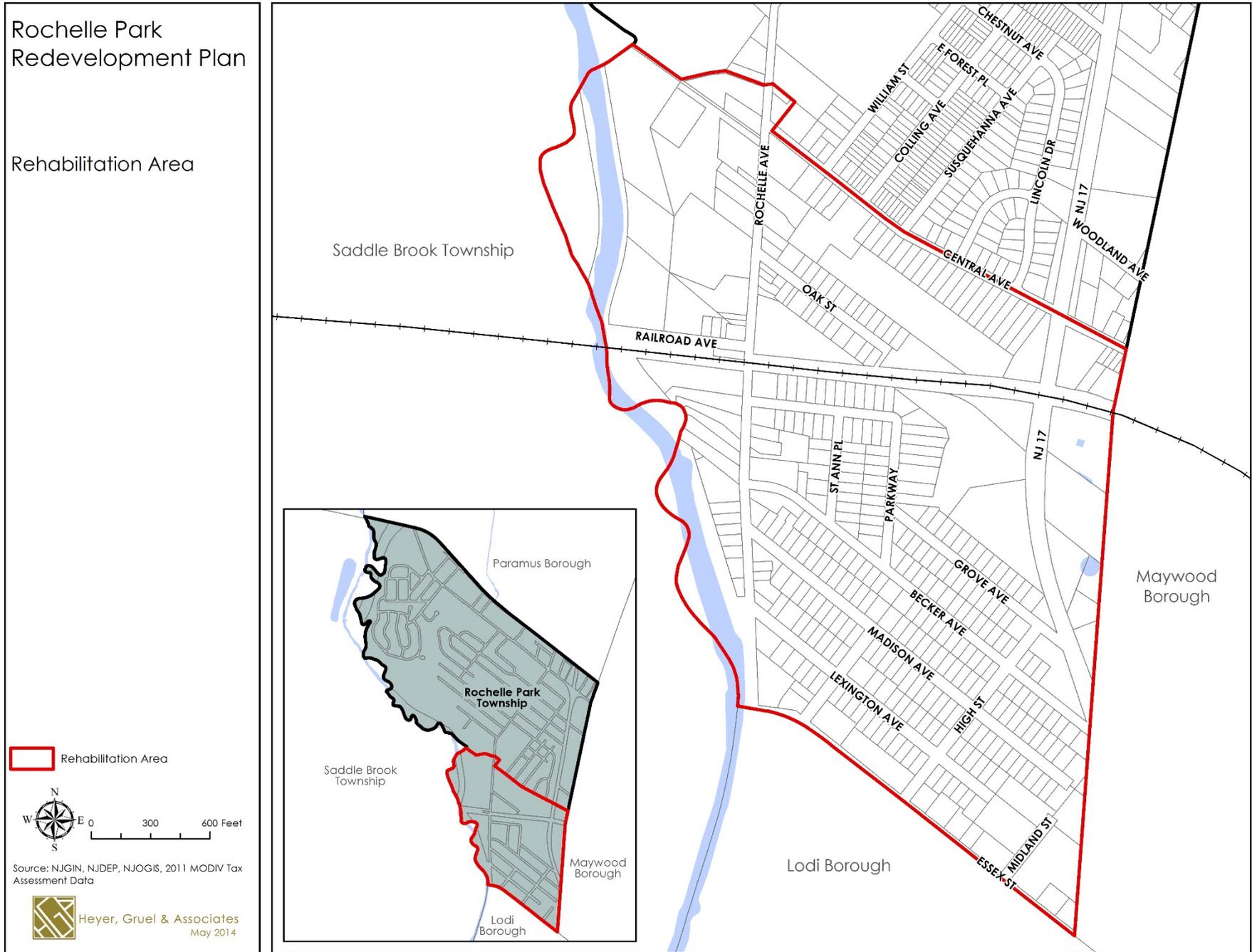
The Rochelle Park Rehabilitation Area comprises the southern portion of the Township including 410 lots for a total of 119.6 acres. Within the Area there are two residential neighborhoods that are separated by an active freight railroad line. There is also a neighborhood commercial area along Rochelle Avenue that includes retail, mixed use development, and office uses.

Existing Land Use in Redevelopment Area			
Category	Acreage	Percentage	No. of Parcels
Vacant	4.1	3.4%	13
Residential	52.5	43.9%	328
Commercial	31.8	26.6%	41
Industrial	3.1	2.6%	6
Apartment	4.8	4.0%	2
Railroad Class I	0.1	0.1%	1
Public Property	24.3	20.3%	14
Church & Charitable	0.7	0.6%	1
Other Exempt	1.5	1.3%	4
Total	119.6	100.0%	410

Source: 2013 MOD IV Tax Assessment Data. The acreage was calculated using Geographic Information Systems and does not constitute a land survey.

Within the area there is a municipal facility including the Fire, Police, and Public Works departments of the Township. Saddle River County Park is located along the Saddle River, in the western section of the area.

Within the area there are 328 residential lots which account for 52.5 acres or 43.9% of the land area. Also within the Rehabilitation Area are 41 commercial lots, which account for 31.8 acres or 26.6% of the land area, 6 industrial lots, which account for 3.1 acres or 2.6% of the land area, and 14 publicly owned lots, which account for 24.3 acres or 20.3% of the land area.



Existing Conditions



Railroad Avenue Office Building



Railroad Avenue Commercial



Rochelle Avenue Commercial



Rochelle Avenue Office Building



Rochelle Avenue Converted Residential Commercial Use



Intersection of Rochelle Avenue and Central Avenue



Rochelle Avenue Gas Station



Rochelle Avenue Commercial



Rochelle Avenue Streetscape

STATUTORY REQUIREMENTS

According to the Local Redevelopment and Housing Law (N.J.S.A. 40A:12A-1, et seq.), the Redevelopment Plan shall include an outline for the planning, development, redevelopment or rehabilitation of the project area sufficient to indicate:

1. Its relationship to definite local objectives as to appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements;
2. Proposed land uses and building requirements in the project area;
3. Adequate provision for the temporary and permanent relocation as necessary of residents in the project area including an estimate of the extent to which decent, safe and sanitary dwelling units affordable to displaced residents will be available to them in the existing local housing market;
4. An identification of any property within the redevelopment area proposed to be acquired in accordance with the redevelopment plan;
5. Any significant relationship of the redevelopment plan to:
 - a. The master plans of contiguous municipalities;
 - b. The master plan of the County in which the municipality is located; and
 - c. The State Development and Redevelopment Plan adopted pursuant to the “State Planning Act” PL 1985, C398 (C52:18A-196 et al.).
6. As of the date of the adoption of the resolution finding the area to be in need of redevelopment, an inventory of all housing units affordable to low and moderate income households, as defined pursuant to section 4 of P.L. 1985 c.222 (C.52:27D-304), that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan.

PLAN PRINCIPLES

Commercial Corridor

- Promotion of the infill, assembly, and expansion of the commercial corridor along Rochelle Avenue that will include retail, office, and mixed use buildings.
- Creation of a walkable and pedestrian friendly commercial environment with sidewalks and traffic calming techniques.
- Incorporation of graduated density guidelines and incentive zoning approaches along Rochelle Avenue to create a more comprehensive neighborhood commercial environment.
- Coordination of the rehabilitation and redevelopment of vacant and underutilized sites along Rochelle Avenue.
- Construction of well- designed integrated mixed use, retail, and office developments.
- Provisions for adequate parking through conventional and creative strategies.

Residential Areas

- Rehabilitation of aging housing within the Rehabilitation Area.

- Continuation and expansion of the Township’s rehabilitation program with the County.

Transportation and Parking

- Reduction of congestion along Rochelle Park through the commercial corridor. The needs of cyclists and pedestrians should be factored into roadway redesign. Traffic calming techniques and traffic signal timing should also be considered.
- Provision for on-street parking along Rochelle Avenue through the commercial corridor.
- Provision for sufficient parking on site for residents, new and existing.

Site Design

- Creation of connections between the Rochelle Avenue commercial corridor and the surrounding residential neighborhoods and Saddle River County Park.
- Consideration of context through site development including future development along Rochelle Avenue.

- Incorporation of the redevelopment of municipal facilities into future potential developments.

Economic Incentives

- Creation of a package of economic incentives that spur development of mixed use projects along the Rochelle Avenue commercial corridor.
- Creation of zoning incentives to assemble parcels such that the economic return on assembled parcels is significantly higher than parcels developed individually.
- Alignment of the Township's housing rehabilitation program with the County's Home Improvement Program to help homeowners renovate, rebuild, and revitalize the aging housing stock located within the Area.

GOALS AND OBJECTIVES

- To improve the Rochelle Avenue commercial corridor with new mixed use development that will be compatible with the uses, scale, and intensity of the surrounding area.
- To address the supply and location of parking, and present and future parking needs of the area businesses and residences.
- To create a circulation pattern that will serve the proposed uses while reducing congestion on Rochelle Avenue, Central Avenue, and Railroad Avenue.
- To encourage developers to meet sustainable/ green standards.
- To improve the utilization of land which can be more effectively redeveloped and/ or rehabilitated for community benefit.
- To promote economic development and broaden the Township's tax base.
- To create a "gateway" into the Rochelle Avenue commercial corridor.
- To create connections between the commercial corridor, adjacent residential neighborhoods, and the Saddle River County Park.
- To encourage mixed use developments by the use of graduated density incentives.
- To provide for a safe bicycle and pedestrian environment within the residential neighborhood and along Rochelle Avenue, Central Avenue, and Railroad Avenue.
- To incorporate the redevelopment of municipal facilities into future redevelopment plans.
- To create a framework for rehabilitating the Area's aging housing stock.
- To utilize a Form Based Code Option to provide understandable and predictable developments.
- To preserve and maintain the existing predominantly single family neighborhoods.

RELATIONSHIP OF PLAN TO TOWNSHIP LAND DEVELOPMENT REGULATIONS

The Area shall be redeveloped in accordance with the standards detailed in this Redevelopment Plan. This Plan includes a form based code option for the commercial corridor which shall constitute an overlay for the current zoning. The developer shall have the option of conventional zoning or the form based code option, but once chosen the options cannot be combined or altered. The areas outside the commercial corridor shall be subject to conventional zoning.

Any deviation from permitted use standards, or height of a principal structure standards which would result in a “d” variance shall be addressed as an amendment to the Plan. Neither the Planning Board nor the Board of Adjustment shall have authority to allow deviations which would result in a “d” variance. The Planning Board shall have power to grant relief from other bulk and dimensional requirements of this Plan to the same extent as the Board may grant relief from bulk and dimensional requirements pursuant to the N.J.S.A. 40:55D-70c.

All exceptions or waivers from design standards from the requirements for site plan or subdivision approval shall be within the jurisdiction of the Township Planning Board.

All development, with the exception of one and two family homes where permitted, must be approved by the Planning Board and shall be submitted through the normal site plan and subdivision procedures as identified by N.J.S.A. 40:55D, et seq.

Final adoption of this Redevelopment Plan by the Township Committee shall be considered an amendment to the Township Zoning Ordinance and Official Zoning Map.

Unless otherwise noted, definitions in this Plan are consistent with the Township’s Zoning and Land Use Ordinance.

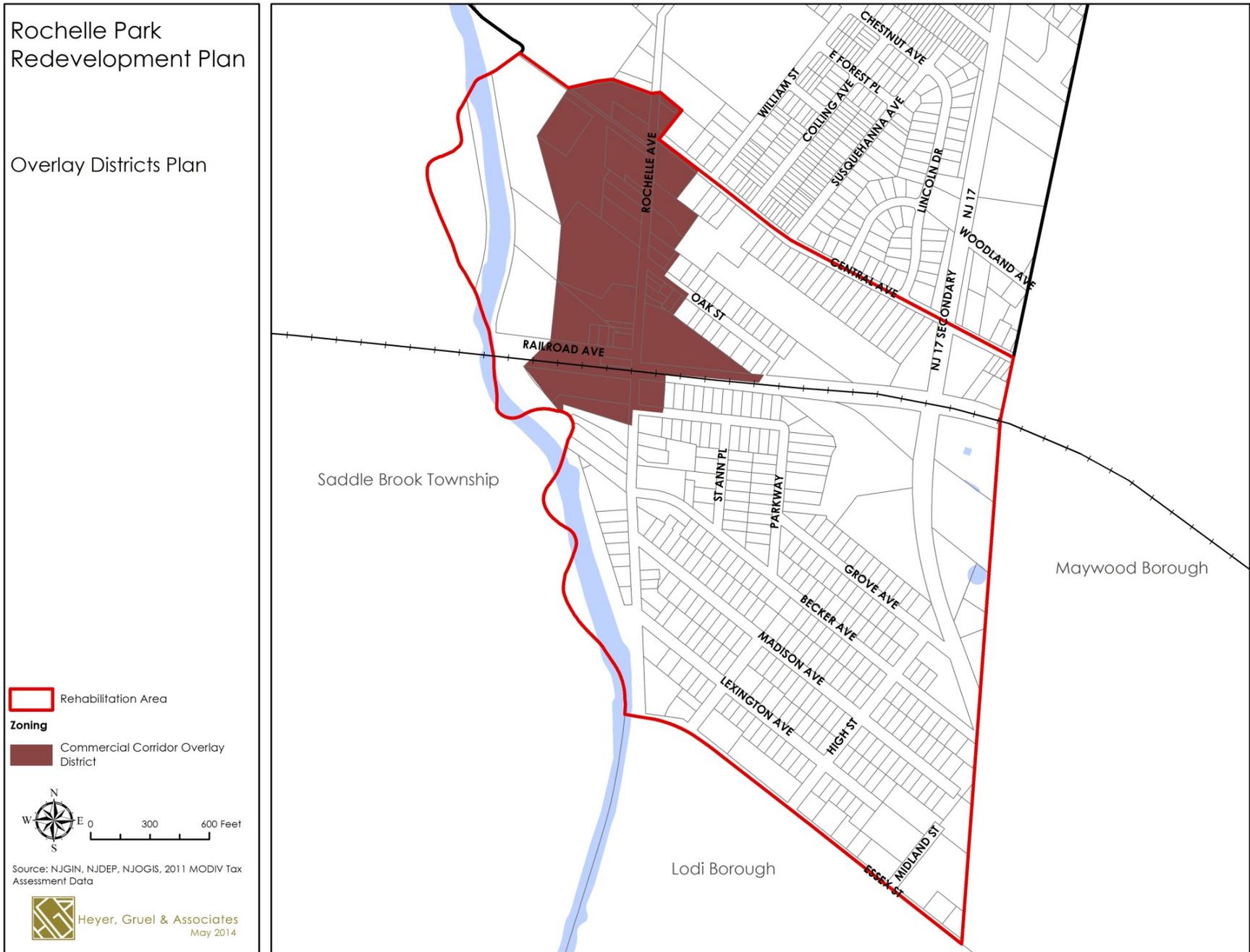
LAND USE PLAN

The following district constitutes an overlay to the existing zoning within portions of the Rehabilitation Area (see attached Overlay District Map). The remaining Area is controlled by the existing zoning. The overlay zone is the Commercial Corridor Overlay District.

Within the commercial corridor overlay district, developers have the option of using the existing zoning or the Form Based Code Option, outlined in the Form Based Code Option section of this Plan.

Rochelle Park
Redevelopment Plan

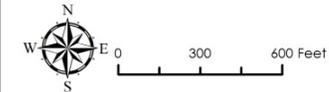
Overlay Districts Plan



Rehabilitation Area

Zoning

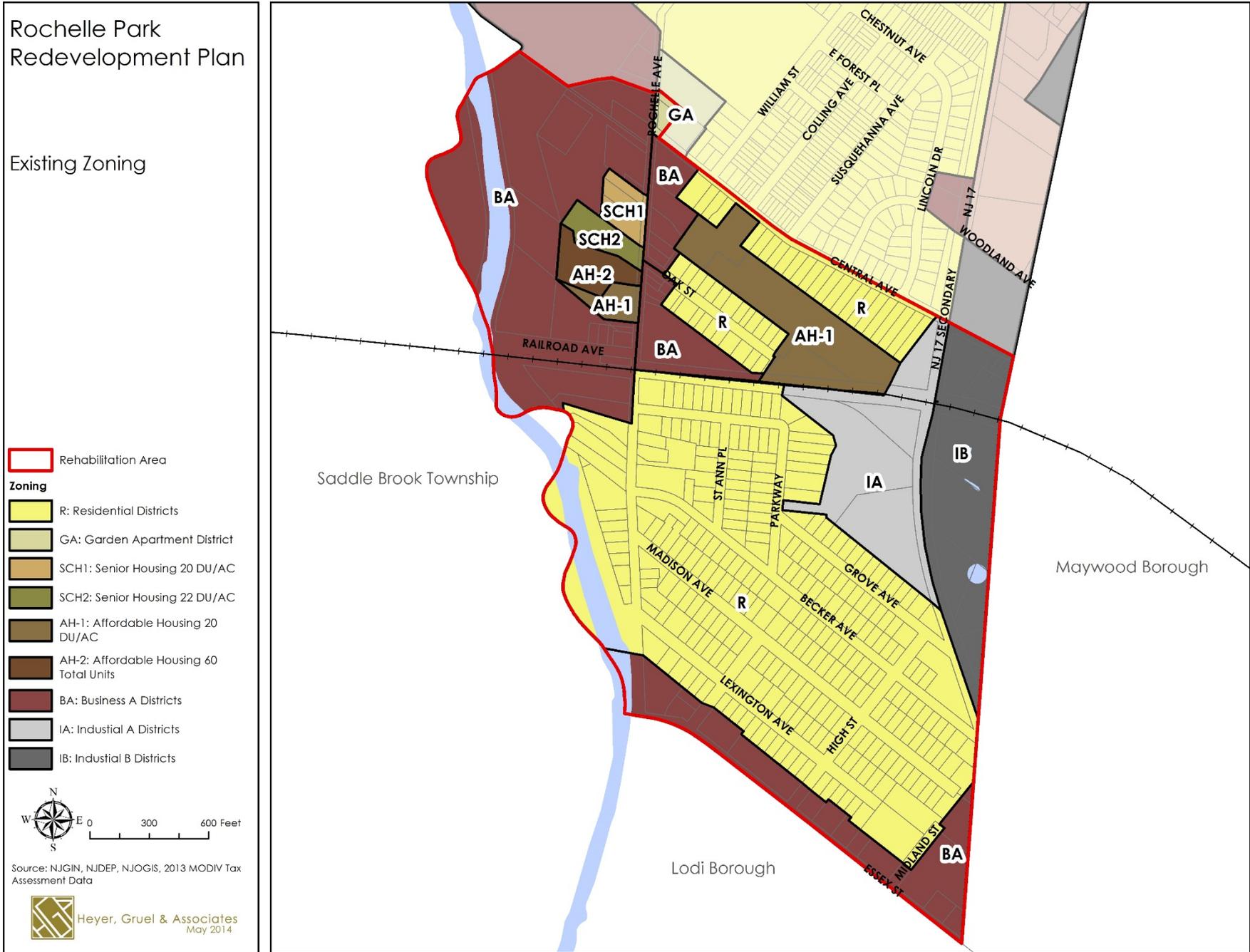
Commercial Corridor Overlay District



Source: NJGIN, NJDEP, NJOGIS, 2011 MODIV Tax Assessment Data



Heyer, Gruel & Associates
May 2014



FORM BASED CODE OPTION

This Redevelopment Plan lays out a framework for future redevelopment within the Commercial Corridor Overlay District. Potential developers may choose to either develop the parcels based on existing zoning or the Form Based Code Option. Once a choice has been made, no deviations or merging of the two options will be permitted.

The Form Based Code Option within the Commercial Corridor Overlay District is a graphic depiction of allowable building features and zoning requirements.

The Form Based Code Option stipulates how the building will be massed, the approved materials to be used, how developments will relate to the street, as well as other features that will guarantee a development that achieves the goals of this Plan.

The Form Based Code Option is structured in three parts including a regulating plan, building typologies, and street-corridor plan.

Regulating Plan

The Regulating Plan identifies how the building typologies relate to each other and where they are permitted to be placed.

Building Typologies 1 (Mixed Use: Retail/ Office) and 2 (Mixed Use: Retail/ Residential) may be used throughout the Commercial Corridor Overlay District north of Railroad Avenue.

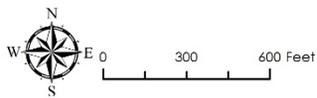
Building Typology 3 (Residential) may be used throughout the Commercial Corridor Overlay District only on lots that are between 12,000 square feet to 20,000 square feet in size.

Building Typologies 4 (Office) and 5 (Retail) are only permitted on the parcels located at the intersection of Rochelle Avenue and Railroad Avenue. Building Typology 4 (Office) can also be used on parcels south of Railroad Avenue.

Rochelle Park
Redevelopment Plan

Regulating Plan

-  Typologies 1, 2, & 3
-  Typologies 3 & 4
-  Typologies 4 & 5
-  Rehabilitation Area



Source: NJGIN, NJDEP, NJOGIS, 2013 MODIV Tax Assessment Data

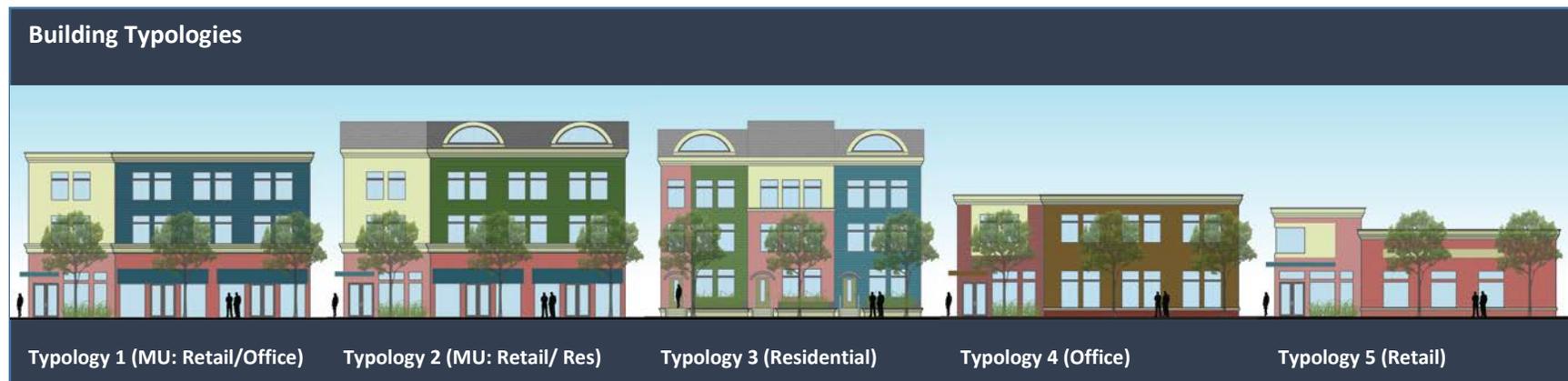


Building Typologies

Within the Commercial Corridor Overlay District five building typologies were created to achieve the goals laid out within this Plan. The five building typologies include mixed use- retail/office, mixed use- retail/residential, office only, residential only, and retail only typologies.

Any deviations from the bulk standards within the building typologies will result in a “C” variance.

The following standards as it relates to additional setback and parking apply to all typologies.



Parking Standards

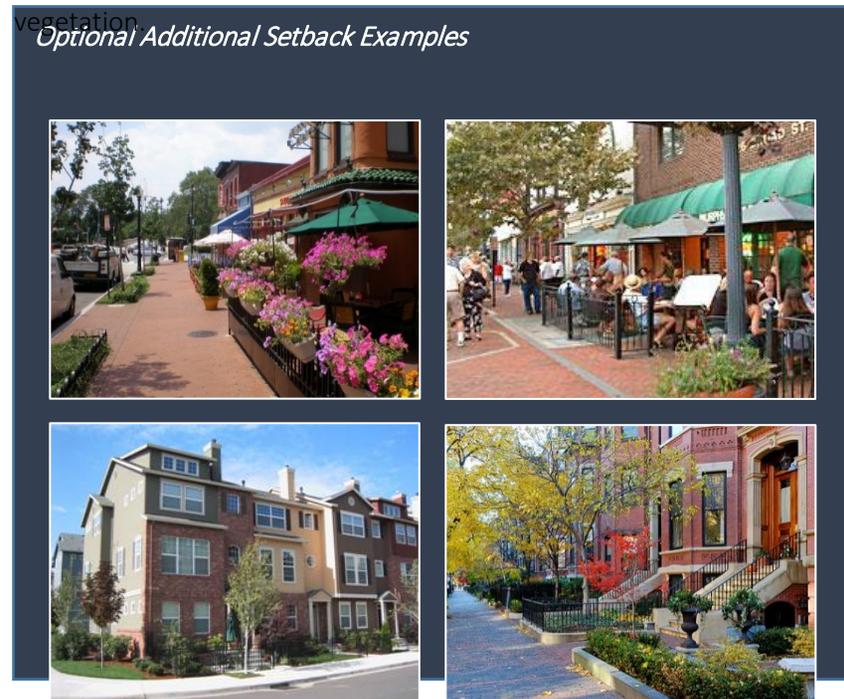
The following parking standards apply to parcels that have an area greater than 12,000 square feet within the Form Based Code Option area. All parcels less than 12,000 square feet in area shall comply with parking requirements in the underlying zone.

- Minimum On-site Parking (> 12,000 square foot lot size)
 - Residential Dwelling Unit: 1.5 spaces per unit
 - Eating and Drinking Establishments: 1 space per 4 dining seats
 - General and Professional Offices: 1 space per 300 square feet
 - Retail Sales and Services: 1 space per 250 square feet of net floor area
 - Banks and Financial Institutions: 1 space per 300 square feet of net floor area
 - Non-residential uses (except for Eating and Drinking Establishments, general and professional offices, retail sales and services, and banks and financial institutions): 1 space per 250 square feet of net floor area
 - A shared parking model may be employed using methods described in “Shared Parking” published by the Urban Land Institute.

- If on street parking is created as a part of the project, it may be counted toward the requirement.
- Parking lots shall be set back a minimum of 5’-0” from the rear and side yards. A buffer area consisting of both vegetation, such as hedges or shrubs, and approved fencing is required between all parking lots and property lines.

Optional Front Setback

An optional front setback may be granted of no more than 10’-0” for an active use or approved passive use. An active use includes street cafes, building corner conditions/ entrances. Passive uses are restricted to residential only buildings and include fenced gardens and



Building Typology 1: Mixed Use- Retail/ Office

Description:

This building typology includes two- three stories, ground floor retail or office use and offices on upper stories.

Mixed Use buildings at the scale of 2-3 stories offer a “human scale” while activating the streetscape. Ground floor retail shops, eateries, or neighborhood services like dry-cleaners, create an inviting and friendly commercial corridor. The second and third story offices are meant to be rented or owned by small businesses or professional firms such as independent accountants, lawyers, and medical practices. The scale of this building typology blends well with the adjacent residential neighborhoods by increasing the density along the commercial corridor but not dwarfing adjacent neighborhood buildings. High fenestration levels at the ground floor allow for window shopping during the day and lighting for the streetscape at night creating a friendly and inviting environment.

Principal Permitted Uses:

First Floor Only: Child Care Centers; Banks and Financial Institutions; Eating and Drinking Establishments; Retail Sales and Services; Health clubs. Drive-thru uses are not permitted, except for banks and pharmacies located in the rear yard only.

All Floors: Professional and General Offices; Governmental Uses

Permitted Accessory Uses:

Parking; Signage; Any other uses customarily incidental and accessory to the principal use.

Additional Requirements

Each individual use on the ground floor is required to have its own primary entrance on the street/ right of way.

Bulk Standards:

<i>Minimum Lot Size:</i>	12,000 sf
<i>Maximum Height:</i>	3 stories, 38 ft.
<i>Minimum Height:</i>	2 Stories
<i>Max/Min Front Setback:</i>	0’-0”
<i>Optional Front Setback:</i>	10’-0”
<i>Min Rear Setback:</i>	20’-0”
<i>Min Side Setback:</i>	10’-0”
<i>Min Ground Floor Height:</i>	13’-0”
<i>Max Ground Floor Height:</i>	15’-0”
<i>Max Façade Unit Width:</i>	50’-0”
<i>Minimum Retail Requirement:</i>	50% of ground floor area and no less than 1,500 sf

Building Typology 1: Mixed Use- Retail/ Office



Building Typology 2: Mixed Use- Retail/ Residential

Description:

This building typology includes two- three stories with ground floor retail and residential uses and residential on above stories.

Ground floor retail shops, eateries, neighborhood services, and/ or residences create an inviting and friendly commercial corridor. Upper stories are residential only.

Principal Permitted Uses:

First Floor Only: Child Care Centers; Banks and Financial Institutions; Eating and Drinking Establishments; Retail Sales and Services; Health clubs. Drive-thru uses are not permitted, except for banks and pharmacies located in the rear yard only.

All Floors: Multi-Family Residential; Townhouse Units; Governmental Uses

Permitted Accessory Uses:

Parking; Signage; Any other uses customarily incidental and accessory to the principal use.

Additional Requirements

Within typologies 2 & 3, an additional ½ story may be constructed above the 3 stories permitted, with a gable or sloped roof. For all sides of the building, the building height shall not exceed 40 feet above average grade.

Each individual use on the ground floor is required to have its own primary entrance on the street/ right of way. Upper level residential uses may share entries.

Residential porches, stoops, private residential terraces and patios shall be a minimum of 2 feet above the adjacent sidewalk elevation except as required for ADA compliance.

Bulk Standards:

<i>Minimum Lot Size:</i>	12,000 sf
<i>Maximum Height:</i>	3 stories, 38 ft.
<i>Additional Height:</i>	½ Story, not to exceed 40 ft.
<i>Minimum Height:</i>	2 Stories
<i>Max/Min Front Setback:</i>	0'-0"
<i>Optional Front Setback:</i>	10'-0"
<i>Min Rear Setback:</i>	20'-0"
<i>Min Side Setback:</i>	10'-0"
<i>Min Ground Floor Height:</i>	13'-0"
<i>Max Ground Floor Height:</i>	15'-0"
<i>Max Façade Unit Width:</i>	50'-0"
<i>Minimum Retail Requirement:</i>	50% of ground floor area and no less than 1,500 sf

Building Typology 2: Mixed Use- Retail/ Residential



Building Typology 3: Residential

Description:

This building typology includes two- three stories, and is residential on all floors.

Apartment buildings and townhouses provide higher densities and allow for more people to activate the street at all hours of the day. These buildings add to the streetscape by having optional garden and terrace areas that are raised and fenced from the sidewalk.

Principal Permitted Uses:

Multi-Family Residential; Townhouse Units

Permitted Accessory Uses:

Parking and any other uses customarily incidental and accessory to the principal use.

Additional Requirements

Within typologies 2 & 3, an additional ½ story may be constructed above the 3 stories permitted, with a gable or sloped roof. For all sides of the building, the building height shall not exceed 40 feet above average grade.

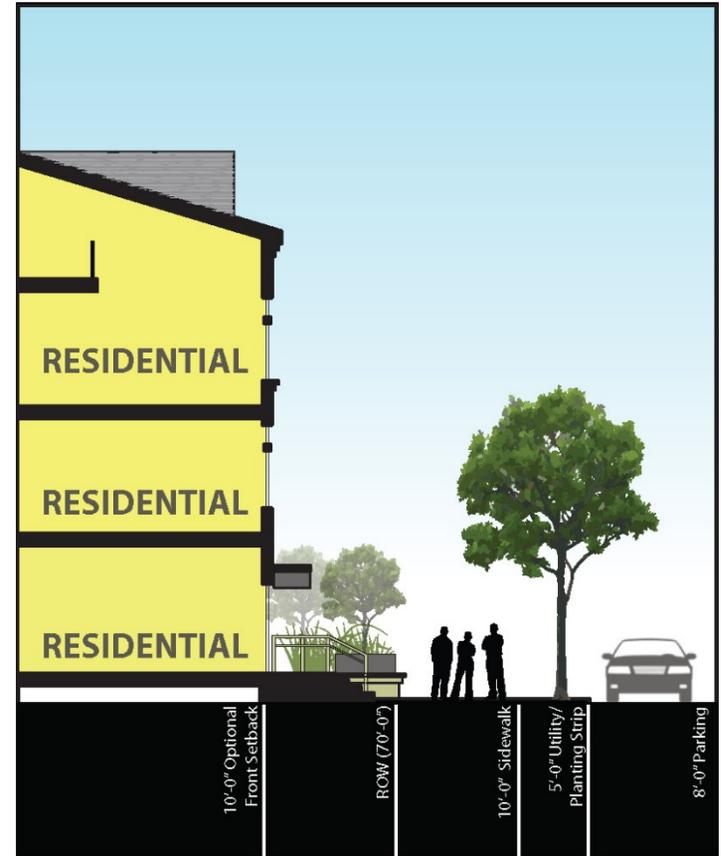
Each townhouse unit is required to have its own primary entrance on the street/ right of way. Upper level apartments may share entries.

Residential porches, stoops, private residential terraces and patios shall be a minimum of 2 feet above the adjacent sidewalk elevation except as required for ADA compliance.

Bulk Standards:

<i>Minimum Lot Size:</i>	12,000 sf
<i>Maximum Height:</i>	3 stories, 38 ft.
<i>Additional Height:</i>	½ Story, not to exceed 40 ft.
<i>Minimum Height:</i>	2 Stories
<i>Max/Min Front Setback:</i>	0’-0”
<i>Optional Front Setback:</i>	10’-0”
<i>Min Rear Setback:</i>	20’-0”
<i>Min Side Setback:</i>	10’-0”
<i>Min Ground Floor Elevation:</i>	2’-0”
<i>Max Façade Unit Width:</i>	50’-0”

Building Typology 3: Residential



Building Typology 4: Office

Description:

This building typology consists of two stories, with office uses on all floors.

Office buildings allow for economic activity and patrons for local shops and restaurants during the day and evening. Offices on the ground floor should be limited and retail uses would be preferred. This typology blends with the existing uses and buildings already present in the district and should be located near or adjacent similar uses within the District.

Principal Permitted Uses:

First Floor Only: Banks and Financial Institutions; Professional and General Office

Upper Floor: Professional and General Office; Governmental Uses

Permitted Accessory Uses:

Parking; Signage; Other uses customarily incidental and accessory to the principal use.

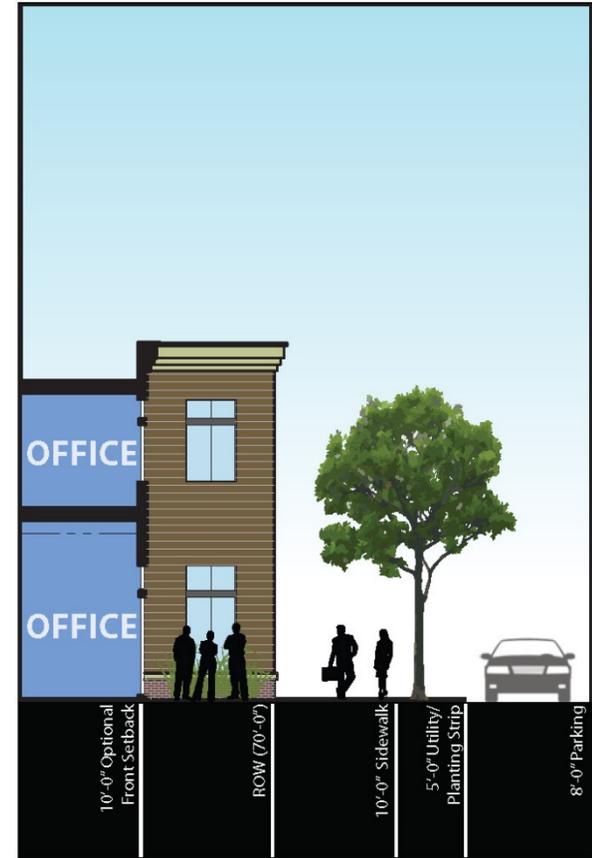
Additional Requirements

Each individual use on the ground floor is required to have its own primary entrance on the street/ right of way. Upper level uses may share entries.

Bulk Standards:

<i>Minimum Lot Size:</i>	15,000 sf
<i>Maximum Height:</i>	2 stories, 30 ft.
<i>Minimum Height:</i>	2 Stories
<i>Max/Min Front Setback:</i>	0'-0"
<i>Optional Front Setback</i>	10'-0"
<i>Min Rear Setback:</i>	20'-0"
<i>Min Side Setback:</i>	10'-0"
<i>Min Ground Floor Height:</i>	13'-0"
<i>Max Ground Floor Height:</i>	15'-0"
<i>Max Façade Unit Width:</i>	50'-0"

Building Typology 4: Office



Building Typology 5: Retail

Description:

This building typology consists of a one story retail structure.

Larger format retail shops and stores are already existing within the Commercial Corridor Overlay District and supply residents of the community with needed resources. These retail services may include small grocery stores, convenience stores, pharmacies, and/or an assortment of smaller retail establishments.

Principal Permitted Uses:

Child Care Centers; Banks and Financial Institutions; Eating and Drinking Establishments; Retail Sales and Services; Health clubs; Essential Services; Governmental Uses

Drive-thru uses are not permitted, except for banks and pharmacies and located in the rear yard.

Permitted Accessory Uses:

Parking and other uses customarily incidental and accessory to the principal use.

Additional Requirements

Each individual use on the ground floor is required to have its own primary entrance on the street/ right of way.

Bulk Standards:

<i>Minimum Lot Size:</i>	25,000 sf
<i>Maximum Height:</i>	1 story, 25 ft.
<i>Max/Min Front Setback:</i>	0'-0"
<i>Optional Front Setback</i>	10'-0"
<i>Min Rear Setback:</i>	20'-0"
<i>Min Side Setback:</i>	10'-0"
<i>Min Ground Floor Height:</i>	13'-0"
<i>Max Façade Unit Width:</i>	50'-0"
<i>Minimum Retail Requirement:</i>	100%

Building Typology 5: Retail



Design Standards

These design standards shall be considered site plan standards. Deviations shall be considered design standard waivers. The design standards apply to any development in the Rehabilitation Area.

General

- New buildings shall relate to public streets and plazas, both functionally and visually. The primary orientation of a building shall not be towards a parking lot or parking structure.
- The appearance of all sides of buildings is important. It may be desirable to develop alternative entries. Therefore, guidelines for the fronts of buildings shall also apply to other sides.
- Ground floor uses of buildings are encouraged to be uses which generate the greatest amount of pedestrian activity, such as retail, restaurants or service uses along Rochelle Avenue, Central Avenue, and Railroad Avenue.
- The type, shape, pitch, texture and color of roof surfaces visible from the street shall be architecturally compatible with the building style, material, colors and details. Roof forms should be similar to those predominantly found on adjacent buildings. New rooftop elements (e.g. HVAC, antennas) shall be screened from the public right-of-way.
- All pedestrian entryways and/or lobbies shall be prominent, well-lit and separate from service entrances.
- In residential development, outside storage of materials and personal belongings of residents shall be prohibited.
- All storage of refuse and recyclable materials shall be maintained within the confines of an enclosed building or structure and shall be reasonably accessible for vehicular collection on the site.
- Every effort shall be made to make utilities as visually unobtrusive as possible.
- Meters and access panels shall be integrated with street and building design.
- To the extent permitted by public utilities, transformers and generators shall be located interior to the building, on the roof or vaulted underground within the pavement area of an adjacent street or sidewalk.

Parking Design

- All 90 degree parking spaces shall be a minimum of 9 feet in width by 18 feet in depth where aisle widths are less than 24 feet in width.
- Aisles shall be a minimum of 22 feet in width.

- Ninety degree parking spaces may be 8 feet, 6 inches in width by 18 feet in depth if the aisle width is a minimum of 24 feet in width.

Architectural Standards

- The exterior walls of buildings shall not have large blank or featureless expanses. No blank walls may be constructed that are larger than 30 feet in length facing a right of way.
- Awnings, canopies, and/or balconies are permitted and shall only extend a maximum of 3 feet from the exterior wall into an established setback.
- The use of real materials, rather than imitations such as brick veneer, is encouraged. Vinyl siding, plastic roof tiles, thin brick veneer or EIFS (Exterior Insulation Finish Systems) are prohibited at ground level and discouraged on upper floors facing public right-of-ways.
- Materials used near sidewalks and adjacent to the entrance shall be durable and compatible with other building materials.
- Air conditioning units should not be placed into windows or any other openings visible from the street. Units located in non-window openings may be permitted if they are screened with a grille within the building wall.

- Foundation exposure shall be limited to a maximum of 36 inches above grade.
- Loading and service areas where required shall be integral to building design and screened from public view. These areas, especially areas used to store garbage, are only allowed in rear yards, and must be fully screened on all sides by fencing and/ or landscaping.
- Chain link fences are not permitted as fencing materials.
- At first floor retail, financial, and food-related uses, at least 60% of their surface area between 3 feet and 8 feet shall be glazed, and the visible light transmittance of that glazing shall be at least 70%.
- The maximum height of the sill above the sidewalk shall be 3 feet for retail, financial and food related uses.
- Exterior security grates are prohibited.
- Openings for windows and windowpanes shall have a vertical dimension greater than or equal to the horizontal dimension.
- Windowsills shall project a minimum of 2 inches from the building face.
- All lintels shall extend a minimum of 4 inches beyond the edge of the opening.
- All hipped or gabled roofs ends shall have eaves. Eaves shall be continuous, unless overhanging a balcony or porch.

- Rooftop appurtenances, including architectural features such as spires, cupolas, domes, and belfries, are permitted, as long as their highest points are no more than 15 feet above the maximum overall height of the building, and as long as the total areas enclosed by the outer edges of the appurtenances, measured at the maximum overall height of the building, does not exceed 10 percent of the total horizontally projected roof area of the building.
- Stairs and elevator penthouses which project above the maximum height of the building shall count toward the 10% allowance. Parapet walls and equipment screens which project above the maximum overall height of the building shall also count toward that 10% allowance.

Green/Sustainable Standards

- Green Sustainable building, design (including LEED) and storm water technology shall be incorporated, to the extent feasible.

Street-Corridor Plan

Rochelle Avenue is a county road and therefore falls under the jurisdiction of Bergen County. Currently Bergen County is reviewing a draft Complete Streets Policy that will implement standards that safely accommodate all modes of transportation on the County’s roads. The County’s policies currently entail enlarging the right of way along Rochelle Avenue from 50’-0” to 70’-0”. This plan illustrates this widening and establishes design criteria for the corridor.

Complete Streets policies set in place standards for streets that take into consideration all modes of transportation along a corridor. Differing from traditional street design, complete streets emphasize and design for all modes, including pedestrians, bicycles, buses and mass transit, and automobiles. Common recommendations include bike lanes, sharrows (shared bike lanes), center medians, curb extensions, wide sidewalks, and cross walks.

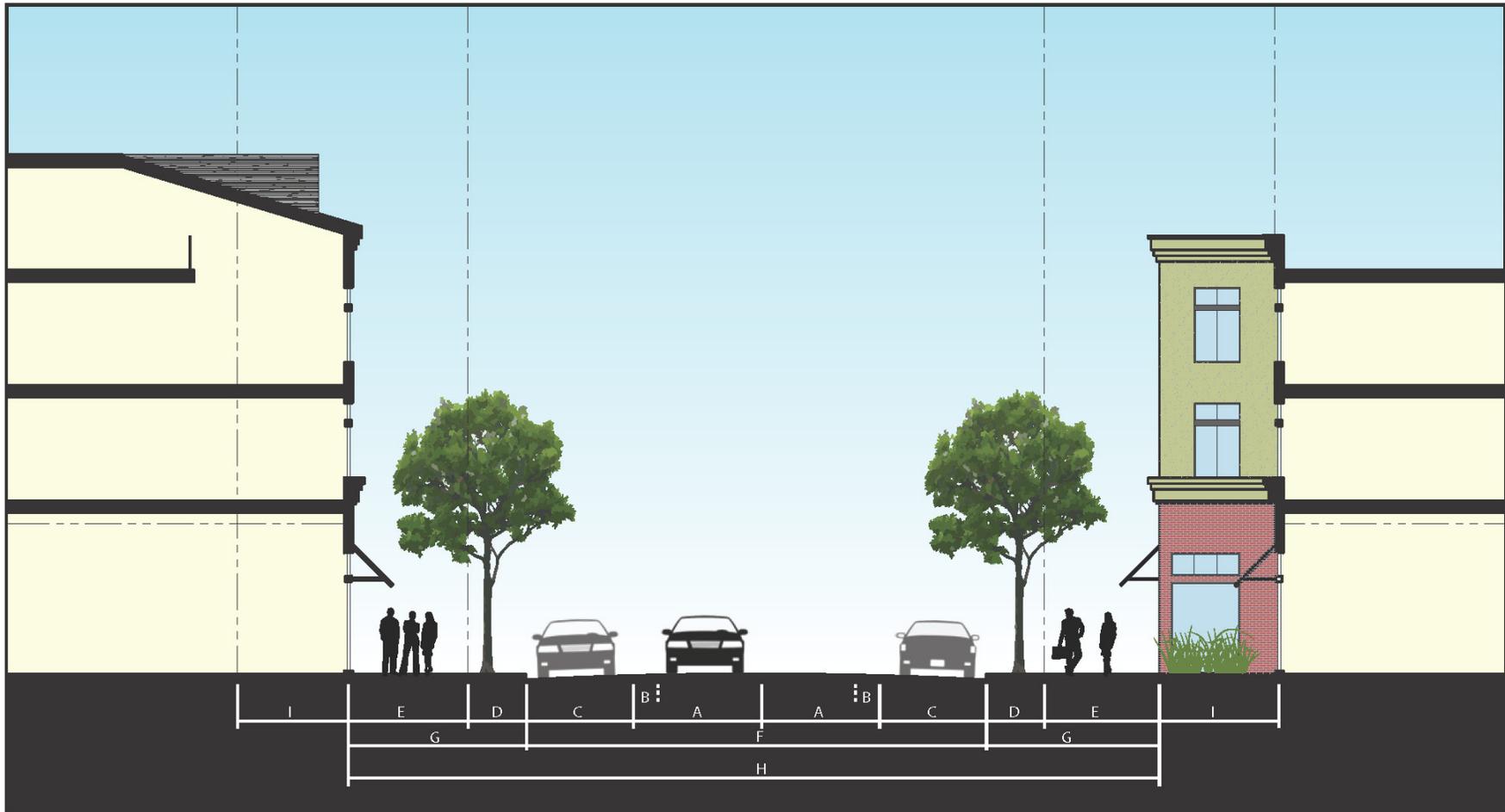
In line with the County’s goals of establishing a Complete Streets Policy, the Street- Corridor Plan outlines recommendations for how Rochelle Avenue, the main thoroughfare through the Commercial Corridor Overlay District can be transformed into a “complete street” that makes accommodations for all modes of transportation and achieves the goal of create a friendly commercial environment for the Township of Rochelle Park.

The Township will have to work with the County on an implementation strategy to make sure the plan for Rochelle Avenue is in line with the County’s Complete Street’s Policy once it is approved.

Existing Streetscape Conditions



Street Design Elements



A. Traffic Lanes	11'-0" to 12'-0"	F. Cartway	40'-0" to 49'-0"
B. Shared Lane Markings	11' from Curb	G. Pedestrian realm	11'-0" to 15'-0"
C. Parallel Parking	8'-0"	H. Right Of Way	70'-0"
D. Tree Wells/ Utilities	4'-0" to 5'-0"	I. Optional Front Setback	10'-0"
E. Sidewalk	7'-0" to 10'-0"		

Curb Extensions

Curb extensions are a type of traffic calming technique that is commonly used in commercial areas to minimize pedestrian crossing distances and thereby reducing wait times for turning vehicles. Curb extensions if designed properly, will not interfere with vehicular turning radii and allow for additional space within the public realm for street furniture, vegetation, or storm water management. Proposed corner conditions are illustrated on the following page.

Sharrows

Shared Lane markings are commonly referred to as sharrows and are an approved Uniform Traffic Control Device in the current FHWA Manual. Sharrows may be used on streets with speed limits at or below 35mph and placed on center 11'-0" from the curb when parallel parking is present, 3'-0" when it is not.

Along Rochelle Avenue, sharrows may be used to guide bicyclists to appropriate entries to Saddle River County Park and residential neighborhoods along the corridor.

Sidewalks

- Sidewalks shall be provided to all entrances of all buildings, which are set back from street.
- Sidewalks shall be constructed of solid and durable materials, such as poured concrete, concrete pavers or stone, according to industry standards.
- Curbing shall be constructed of either granite, concrete or Belgium block with a minimum exposed face of 6 inches wide along all streets.
- A 4 feet wide paver strip shall be provided interior to the curb.

Curb Extension Examples



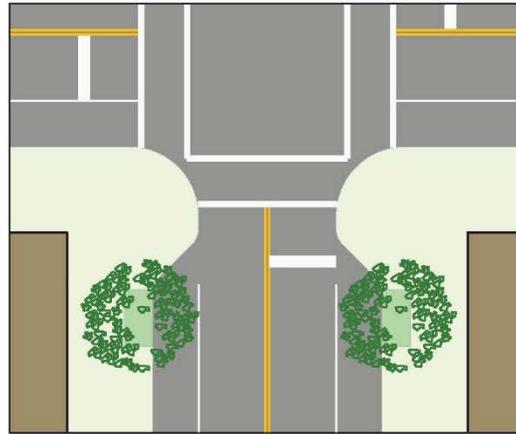
Sharrow Example



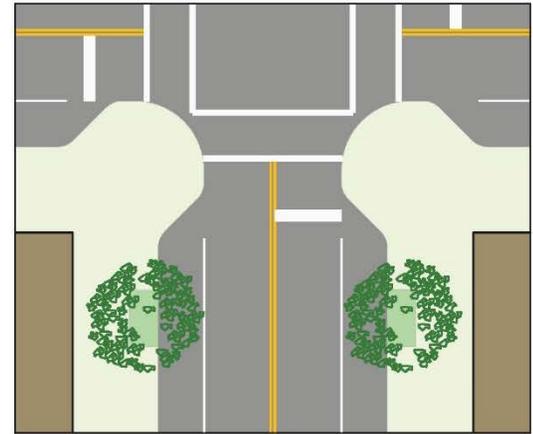
Corner Conditions



Existing Corner Condition



Proposed Curb Extension Option 1



Proposed Curb Extension Option 2

Streetscape and Corner Condition Examples



Street Lighting: Recommended Fixture and Pole



Street Lighting

- Light fixtures shall be mounted at 18 feet; the supporting light pole shall not exceed 20 feet in height.
- Illumination shall be consistent throughout the Area.
- Street lights shall be coordinated with other street furniture and street trees.
- Street lights shall be mounted on fixtures which arc towards the ground plane.
- The recommended PSEG approved manufacturer is Holophane Lighting. The recommended style is GlasWerks Hallbrook or equivalent.

Street Furniture

Street furniture is designed to provide visual unity, elegance and calm to the streetscape. The quality and placement of street furniture should enhance the pedestrian experience and reinforce the overall design theme of the Area. Toward this goal, street furniture, unless noted, shall be constructed of stainless steel or brushed aluminum.

In general, street furniture should be organized to be both functional and attractive. Street furniture should be regularly spaced and aligned with the street trees and street lights, whenever possible. Elements should be placed in logical locations, such as near major

entranceways or crosswalks. Street furniture should in no way inhibit circulation.

The following are recommended styles and suppliers of street furniture, as well as specific details and allocation guidelines.

- Benches
Benches shall be approximately 72 inches in length and 32 inches high. Manufacturer shall be Landscape forms “Plainwell” model or equivalent. Benches are encouraged in

appropriate locations along Rochelle Avenue and Railroad Avenue. Benches shall be located adjacent to the curb line. Benches shall be secured permanently.

- Tree Grates
Tree grates are required for all street trees. Tree grates shall be 4 feet x 4 feet and set parallel and flush with the curb line. Tree grates shall be made of heavy grade, cast iron or cast aluminum of black or dark gray finish or recycled polygrate plastic. The center hole shall be expandable to accommodate tree growth. The initial center hole shall be a minimum of 12 inches in diameter. The manufacturer shall be Urban Accessories “COHO” style or an equivalent.
- Trash & Recyclable Receptacles
Trash and recyclable receptacles at a minimum, are required at each corner of all intersections. Trash receptacles shall be 24 or 36 gallon, black, and supplied by Victor Stanley, Inc. or an equivalent.
- Bicycle Racks
Bicycle racks are required along Rochelle Avenue and located where appropriate. Bike racks shall be securely fastened to the sidewalk. Bike rack locations shall not interfere with pedestrian circulation. Bike racks shall be black and supplied by Victor Stanley, Inc. or an equivalent.

Street Furniture: Recommended Styles



Street Trees

Street Trees and plantings are an important component of the streetscape and are designed to give character to the Commercial Corridor Overlay District. Street trees are to be coordinated and maintained as an urban forest to improve air and water quality, reduce noise and light pollution and promote the health and well-being of people. Trees shall not be within the site triangles of intersections or driveways. All street trees shall have a minimum diameter of 2.5 inches measured three feet above the ground.

Rochelle Avenue, Railroad Avenue and Central Avenue shall be planted with large trees with attractive bark, small, simple leaf and vase like habit, spaced 35 feet apart. Trees include:

- *Ulmus parvifolia* – Chinese Elm
- *Ulmus americana* – American Elm
- *Zelkova serrata* ‘Green Vase’ - Green Vase Zelkova
- *Zelkova serrata* ‘Village Green’ – Village Green Zelkova

OPPORTUNITY SITES

Within the Commercial Corridor Overlay District along Rochelle Avenue, three opportunity sites have been selected for further analysis. For each opportunity site, a concept plan has been developed using the Form Based Code Option standards laid out in this plan. The code is intended to provide incentives for assembly of lots. In general the code permits greater diversity of uses and allows for more intense development than the underlying conventional zoning. This section is intended to be illustrative of some of the potential development that could be produced by this plan.

Existing Conditions and Development Potential

Within the Commercial Corridor Overlay District the existing parcels were identified as “opportunity sites”. The sites identified in this section are a merger of several parcels that if not merged would result in very limited development potential.

Opportunity Site 1 is located at the intersection of Railroad Avenue and Rochelle Avenue. Currently Opportunity Site 1 consists of eight parcels with several uses including residential, retail, and office. Five of the eight parcels are less than 5,000 square feet and are

configured at odd angles. Seven of the eight parcels are located in the Business A zoning district. This district allows for retail sales and services, eating and drinking establishments, offices and other related uses. No minimum lot area, lot width, or lot depth is established in this district resulting in the small and awkwardly configured lots. One parcel is located in the Affordable Housing 1 zoning district which allows for residential uses and professional offices.

Due to the configuration of these lots, the proposed Form Based Code Option allows for this site to be reconfigured with minimum lot area defined and a commercial corridor atmosphere created through appropriate setbacks and streetscape standards. The Form Based Code Option also allows for mixed use buildings, office buildings, retail establishments, or residential structures to be built thereby increasing the density of the development opportunity, while maintaining a relationship and a continuity with the existing surrounding neighborhoods.

Opportunity Site 2 is located at the intersection of Oak Street and Rochelle Avenue. The site is made up of seven parcels all less than

5,500 square feet in size. On these parcels are a range of uses including retail, residential, and office. Five parcels are located in the Business A zoning district, and two parcels are located in the Residential zoning district. The lots within this site are small and configured resulting in awkward angles similar to Opportunity Site 1.

The Form Based Code Option on the site results in the opportunity to have mixed use buildings or a multi-family residential structure. By establishing a minimum lot area, and as a result merging the parcels, the development opportunities on the site become much greater allowing for higher density buildings in line with the commercial corridor district goals and cohesive with the surrounding neighborhood.

Opportunity Site 3 is located at the intersection of Central Avenue and Rochelle Avenue. Currently the site is made up of two parcels, one with a gas station and the other with a multi-family residential structure. The site is within the Garden Apartment zoning district for single and multi-family residential dwellings.

The site already holds significant potential due to its corner location along Rochelle Avenue. To utilize the Form Based Code Option, a minimum lot size would be required, thereby encouraging the merging of the two parcels. This would allow for higher density and mixed use and/or residential structures to be built.

**ROCHELLE PARK
REDEVELOPMENT PLAN**

Opportunity Sites
Existing Conditions



HEYER, GRUEL & ASSOCIATES
Community Planning Consultants
March 2014



**ROCHELLE PARK
REDEVELOPMENT PLAN**

Opportunity Sites

Site 1:

- Lot 1: 58,100 s.f.
1.3 acres
- Lot 2: 64,960 s.f.
1.5 acres

Site 2:

- Option A: 25,330 s.f.
0.6 acres
- Option B: 17,360 s.f.
0.4 acres

Site 3:

- Lot 1: 13,840 s.f.
0.3 acres



Opportunity Site 1

Opportunity Site 1 is located at the corner of Rochelle Avenue and Railroad Avenue. Currently the site is an array of parcels with varying uses including residential apartments, retail, offices, and single family houses.

The proposed would reorganize the parcels into two parcels by exchanging land as well as consolidating smaller parcels. This enables both properties to meet the minimum lot size established by the Plan.

On the existing parcel, which would be reconfigured by exchanging land, the existing office building would remain. The parking lot would be reconfigured to gain parking efficiency and capacity.

On the proposed consolidated parcel, the building footprint would be pushed to the corner of the property creating a preferred corner condition at the intersection of Railroad Avenue and Rochelle Avenue. Allowed uses include all of the typologies. Using Option A, a mixed use ground floor retail, office/ residential above building is illustrated with parking in the rear. The optional additional setback is used at the corners of the building for active retail and café spaces along the streetscape.

Using Option B, an office/ retail or retail only building is illustrated. The proposed footprint is reminiscent of the office building that is

adjacent, however it is pushed to the corner for a preferred corner condition at the intersection of Railroad Avenue and Rochelle Avenue. The primary entrance for the retail component would preferably be located at the corner of the two streets with active retail space along the streetscape. Parking for this option would be located in the rear and to the side of the building.

A shared parking agreement between the two proposed reorganized parcels might increase the potential development opportunities on the sites and offer the ability to eliminate a portion of the large parking lots needed for the proposed uses.

**ROCHELLE PARK
REDEVELOPMENT PLAN**

Opportunity Sites

Site 1, Option A

Lot Area:
Lot 1: 58,100 s.f.
1.3 acres
Lot 2: 64,960 s.f.
1.5 acres

Proposed:
Existing Office (Lot 1)
3.5 Story Mixed Use
Retail/ Residential (Lot 2)

Retail: 9,450 s.f.
Residential: ~40 units

Parking:
Lot 1: 95 spaces
Lot 2: 88 spaces



Opportunity Site 1, Option A Vision



ROCHELLE PARK REDEVELOPMENT PLAN

Opportunity Sites

Site 1, Option B

Lot Area:

- Lot 1: 58,100 s.f.
1.3 acres
- Lot 2: 64,960 s.f.
1.5 acres

Proposed Option B-1:

- Existing Office (Lot 1)
- 1 Story Retail (Lot 2)

Retail: 14,390 s.f.

Proposed Option B-2:

- Existing Office (Lot 1)
- 2 Story Mixed Use Retail/ Office (Lot 2)

Retail: 7,200 s.f.
Office: 21,590 s.f.

Parking:

- Lot 1: 95 spaces
- Lot 2: 92 spaces





Opportunity Site 2

Opportunity Site 2 is located at the corner of Rochelle Avenue and Oak Street. Currently the site is composed of an arrangement of small parcels with several uses, including residential, retail, and office.

The proposed would consolidate the smaller parcels into one larger parcel to obtain the minimum lot size required by this Redevelopment Plan.

On the proposed consolidated parcel a mixed use, retail on the ground floor, office/ residential above building is shown in Option A. The building footprint fronts onto Rochelle Avenue. Retail would be preferred to activate the corner condition, however is only required for 50% of the ground floor square footage. Townhouse style residential units would be preferred to be located on the remaining ground floor area.

Parking in Option A is located in the rear with access from Oak Street. On street parking, along Rochelle Avenue would also be preferred to be created for retail parking only.

Using Option B, a residential only building is shown. Opportunity Site 2 would be an ideal location for this typology due to its proximity to the adjacent residential neighborhood. The residential building fronts on to Rochelle Avenue with ground floor townhouse style

units having a primary entrance on the streetscape. Front garden, patio, and stoop conditions add to the streetscape and neighborhood atmosphere.

Parking within Option B would be located under the building at grade in the rear, as well as in the rear of the property.

**ROCHELLE PARK
REDEVELOPMENT PLAN**

Opportunity Sites

Site 2, Option A

Lot Area:
Lot 1-A: 25,330 s.f.
0.6 acres

Proposed:
3.5 Story Mixed Use
Retail/ Residential

Retail: 4,700 s.f.
Residential: ~20 units

Parking:
Lot 1-A: 34 spaces

w/ additional on street along
Rochelle Ave/ Oak Street



ROCHELLE PARK REDEVELOPMENT PLAN

Opportunity Sites

Site 2, Option B

Lot Area:
Lot 1-B: 17,360 s.f.
0.4 acres

Proposed:
3 Story Residential

Residential: ~12 units

Parking:
Lot 1-B: 20 spaces



Opportunity Site 3

Opportunity Site 3 is located at the corner of Rochelle Avenue and Central Avenue. Currently, the site is made up of two parcels that are proposed to be consolidated to meet the minimum lot size requirements. Existing on site is a gas station and 2.5 story residential dwelling.

On the proposed consolidated site, a 2 story mixed use retail/ office or office only building is illustrated. The building is positioned on the site to create a corner condition at the intersection of Rochelle Avenue and Central Avenue. Parking would be located in the rear as well as on Rochelle Avenue for retail uses. A curb extension has been illustrated at the corner to show how it could be implemented successfully, creating a friendly walking environment.

ROCHELLE PARK REDEVELOPMENT PLAN

Opportunity Sites

Site 3

Lot Area:
Lot 1: 13,840 s.f.
0.3 acres

Proposed:
2 Story Mixed Use
Retail/ Office

Retail: 1,500 s.f.
Office: 4,480 s.f.

Parking:
Lot 1: 21 spaces

w/ additional on street along
Rochelle Avenue

 **HEYER, GRUEL & ASSOCIATES**
Community Planning Consultants
March 2014



RELATIONSHIP TO OTHER PLANS

2006 Reexamination Report

The Plan is consistent with and particularly relevant to the following objectives within the Township of Rochelle Park’s 2006 Reexamination Report.

- Objective 3: To ensure the compatible development of different land uses with a view of lessening the cost of development and encouraging the efficient expenditure of public funds.
- Objective 4: To encourage the best possible design for new developments and to protect established neighborhoods.
- Objective 5: To promote the continued maintenance and rehabilitation of the Township’s housing stock and support facilities.
- Objective 10: To provide a variety of commercial uses to serve the shopping, professional and personal service needs for residents and visitors alike.

- Objective 11: To provide for the expansion and redevelopment of office and industrial land uses to support local employment and to broaden the local tax base.
- Objective 12: To promote the improvement of existing transportation routes in a safe and efficient manner.

Bergen County Master Plan

In June 2011 Bergen County undertook a visioning process as a first step in drafting a master plan update. “Vision Bergen” broke the county into three sub-regions. Rochelle Park is located in the Southern Sub-Region. “Vision Bergen” is consistent with a number of goals and recommendations contained in the Plan specifically as it relates to:

- Encouraging mixed use developments and neighborhood character
- Rehabilitating existing neighborhoods
- Creating safe connections to parks and open space
- Encouraging the construction of complete streets.

State Strategic Plan

The Final State Strategic Plan, released in October of 2011, outlines goals and principles for targeting investment and growth in the State of New Jersey. This Redevelopment Plan is consistent with the stated goals and objectives of the State plan. Specifically, this Plan is exemplary of the guiding principle of ‘spatial efficiency’, which will help guide state decision making. “Spatial Efficiency: The State of NJ will place value on the economic, social and environmental benefits of investing in areas where infrastructure already exists in an effort to control long-term costs of public services, re-invigorate existing communities, and protect important natural resources.”

The State Strategic Plan also identifies Garden State Values, many of which are consistent with this Plan. For instance, Garden State Value #1 is to concentrate and mix uses, while Garden State Value #2 is to prioritize redevelopment and infill development around existing infrastructure.

The State Plan also notes that priority areas targeted for investment and growth should be those which create compact livable communities that will attract business and workers, and efficiently use infrastructure.

Although the specific areas to be targeted as priorities for investment and growth have yet to be named, this Plan fits the

criteria that are recommended by the State Strategic Plan as an area to be targeted for strategic investment.

Property Acquisition

The use of condemnation is not authorized as an “area in need of rehabilitation.”

Relocation

No relocation is necessary.

Affordable Housing

No affordable housing units are identified to be removed as part of the implementation of this Redevelopment Plan.

ADMINISTRATIVE AND PROCEDURAL REQUIREMENTS

Duration of Redevelopment Plan

The Redevelopment Plan shall remain in effect for 30 years from the date of adoption of this Plan by the Township Committee. After that period, the Zoning Ordinance will regulate the development of the site.

Amending the Redevelopment Plan

This Redevelopment Plan may be amended from time to time in compliance with the requirements of law.